



WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY
600 Fifth Street, NW, Washington, DC 20001-2651

AMENDMENT OF SOLICITATION / MODIFICATION OF CONTRACT

1. AMENDMENT/MODIFICATION A004		2. EFFECTIVE DATE November 9, 2017	
3. ISSUED BY PURCHASING SECTION Frederick R. Voellm JGB Room 301-G Department of Procurement		4. ADMINISTERED BY (If other than block 3)	
5. CONTRACTOR NAME AND ADDRESS (Street, city, county, state, and Zip Code)		6. FORM TYPE (Check only one) <input checked="" type="checkbox"/> AMENDMENT OF SOLICITATION NO. <u>IFB CQ17130/FRV</u> DATE <u>October 4, 2017</u> (See block 7) <input type="checkbox"/> MODIFICATION OF CONTRACT/ORDER NO. _____ DATE _____ (See block 9)	
7. THIS BLOCK APPLIES ONLY TO AMENDMENTS OF SOLICITATIONS <input checked="" type="checkbox"/> The above numbered solicitation is amended as set forth in block 10. The hour and date specified for receipt of Offers <input checked="" type="checkbox"/> is extended, <input type="checkbox"/> is not extended. Offerors must acknowledge receipt of this amendment prior to the hour and date specified in the solicitation, or as amended, by one of the following methods; (a) By signing and returning <u>one (1)</u> copies of this amendment; (b) by acknowledging receipt of this amendment on Acknowledgement of Amendments form; or (c) by separate letter or telegram which includes a reference to the solicitation and amendment numbers. FAILURE OF YOUR ACKNOWLEDGMENT TO BE RECEIVED AT THE ISSUING OFFICE PRIOR TO THE HOUR AND DATE SPECIFIED MAY RESULT IN REJECTION OF YOUR OFFER. If, by virtue of this amendment you desire to change an offer already submitted, such change may be made by telegram or letter, provided such telegram makes reference to the solicitation and this amendment and is received prior to the opening hour and date specified.			
8. ACCOUNTING AND APPROPRIATION DATA (If required)			
9. THIS BLOCK APPLIES ONLY TO MODIFICATIONS OF CONTRACTS/ORDERS (a) <input type="checkbox"/> This Change Order is issued pursuant to _____ The Changes set forth in block 10 are made to the above numbered contract/order. (b) <input type="checkbox"/> The above numbered contract/order is modified to reflect the administrative changes (such as changes in paying office, appropriation data, etc.) set forth in block 10. (c) <input type="checkbox"/> This Supplemental Agreement is entered into pursuant to authority of _____ It modifies the above numbered contract as set forth in block 10.			
10. DESCRIPTION OF AMENDMENT/MODIFICATION: IFB CQ17130/FRV "Switch Points and Stock Rails" is amended as follows: 1. The bid due/opening date is extended from 2:00PM EST on Nov 15, 2017 to 2:00PM EST on Nov 17, 2017. 2. Replace the Bid/Price Schedule with the attached Bid/Price Schedule Rev. 1. Additional new drawing identified on the Bid/Price Schedule (ST-TW-37) is attached to this amendment. 3. Attachment A provides responses to questions received from interested vendors. Please read each question and response carefully. Some responses may change the specifications or terms and conditions of the IFB. (Except as provided herein, all terms and conditions of the document referenced in block 6, as heretofore changed, remain unchanged and in full force and effect through the contract period.) END OF AMENDMENT A004			
11. <input type="checkbox"/> CONTRACTOR/OFFEROR IS REQUIRED TO SIGN THIS MODIFICATION AND RETURN <u>ONE (1)</u> COPIES TO ISSUING OFFICE.		<input type="checkbox"/> CONTRACTOR/OFFEROR IS NOT REQUIRED TO SIGN THIS DOCUMENT	
12. NAME OF CONTRACTOR/OFFICE BY _____ (Signature of person authorized to sign)		15. WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY BY <u>F.R. Voellm</u> (Signature of Contracting Officer)	
13. NAME AND TITLE OF SIGNER (Type or print)	14. DATE SIGNED	16. NAME OF CONTRACTING OFFICER (Type or print) Frederick R. Voellm	17. DATE SIGNED Nov 9, 2017

QUESTIONS FROM INTERESTED VENDORS AND WMATA RESPONSES

1. Q: Drawing ST-TW-021 suggests the 16-6 switch point on the No 8 turnout is not actually guarded, please confirm these are guarded points listed in the bid items below:

BID ITEMS				
ST-TW-021, 022, 030, 031, 038	16' 6"	No. 8 LH Guarded Straight Switch Point	2	Ea
ST-TW-021, 022, 030, 031, 038	16' 6"	No. 8 RH Guarded Straight Switch Point	2	Ea
ST-TW-021, 022, 030, 031, 038	16' 6"	No. 8 LH Guarded Curved Switch Point	3	Ea
ST-TW-021, 022, 030, 031, 038	16' 6"	No. 8 RH Guarded Curved Switch Point	3	Ea

R: Drawings ST-TW-021 and 022 represent unguarded No. 8 turnouts with 16'-6" points, which are no longer used at WMATA. Drawings ST-TW-030, 031, 037, and 038 should be used as reference to for No. 8 guarded switch points and stock rails. WMATA uses AREMA standard 26'-0" long guarded switch points for No. 8 guarded turnouts. Drawings ST-TW-021, 022 and all 16'-6" No. 8 Switch Points (Lines 1-4) have been deleted from the Bid/Price Schedule.

2. Q: Specification 3.4.2 seems somewhat contradictory as if a floating heel was required, a rail length would need to be given over and above the geometry length of the point, and no contract drawing seems to depict what is usually classed as a floating heel (ie no bolts going directly through the heel block)

Specification 3.4.2

i) Floating heel block for No. 8 guarded switch will be manufactured as shown in the contract drawings.

j) Guarded switches will have a modified five bolt heel joint assembly to accommodate the stock rail, switch rail and guard rail in accordance with AREMA Plan 221-03 and AREMA Specifications.

R: No. 6 and No. 8 Guarded turnouts have fixed heel blocks according to drawings. No. 10 and No. 15 turnouts (unguarded) have floating heel block according to drawings. Subsection 3.4.2 i should be disregarded and subsection 3.4.2 j should be used as the specification to manufacture switch points and stock rails.

3. Q: Specification 3.4.2 once again suggests a floating heel, yet no drawing seems to detail this and once again if it is a floating heel is required an "additional length" would need to be listed for the bid items shown below.

Specification 3.4.2

h) Floating heel blocks for No. 10 and No. 15 switches will be manufactured as shown in the contract drawings.

BID ITEMS

ST-TW-042, 43	19' 6"	No. 10 LH Straight Switch Point	2	Ea
ST-TW-042, 43	19' 6"	No. 10 LH Curved Switch Point	1	Ea
ST-TW-042, 43	19' 6"	No. 10 RH Straight Switch Point	1	Ea
ST-TW-042, 43	19' 6"	No. 10 RH Curved Switch Point	1	Ea

R: Drawing ST-TW-042 identifies a floating heel at 22' from P.S., and ST-TW-043 shows an additional length of 13' past the 22'-0" switch point, to make a total rail length of 35'. The items listed should have a point length of 22'-0" and a total length of 35'. (Changes are reflected on the Bid/Price Schedule R1).

4. Q: For the bid items listed below, no "21 foot geometry" point drawing seems to exist ?
Please clarify.

ST-TW-042, 43	21'/30'*	No. 10 LH Curved Switch Point	2	Ea
ST-TW-042, 43	21'/30'*	No. 10 RH Curved Switch Point	2	Ea
ST-TW-042, 43	21'/30'*	No. 10 LH Straight Switch Point	2	Ea
ST-TW-042, 43	21'/30'*	No. 10 RH Straight Switch Point	2	Ea

R. These Line Items 13-16 are related to a vendor specific design and are deleted from the Bid/Price Schedule.

5. Q: For the stockrails listed, a lot of them have the word "guard stock" in the description. What does this mean , and what requirements are there other than the stockrail being a discreet piece of rail with an undercut and the length shown ?

R: The term "guard stock" was included because all WMATA No. 6 and No. 8 turnouts are guarded. The curved stock rail has a 132RE guard rail attached to it from the heel block through to the end of the stock rail. This is for identification purposes, please see referenced drawings.

6. Q: Is the length of the undercut required on the stockrails clearly defined for each stockrail ?

R: There is no clearly defined length of the stock rail undercut, but the length should not vary greatly from what is shown on the drawings. The current stock rails for our No. 8 (26') switch points have a 15' undercut and the current stock rails for our 22'/35' switch rails have a 13' undercut.

7. Q: What is the geometry of the "35 foot long No 10 switch points" listed below?

ST-TW-042, 43	35'	No. 10 RH Curved Switch Point	1	Ea
ST-TW-042, 43	35'	No. 10 RH Straight Switch Point	1	Ea
ST-TW-042, 43	35'	No. 10 LH Curved Switch Point	1	Ea
ST-TW-042, 43	35'	No. 10 LH Straight Switch Point	1	Ea

R: Please see drawings referenced in the answer to question 3.

8. Two similar questions received from interested vendors with regard to Section 3.6.2 of the Scope of Work :

Q: Can you clarify exactly what is required for this inspection?

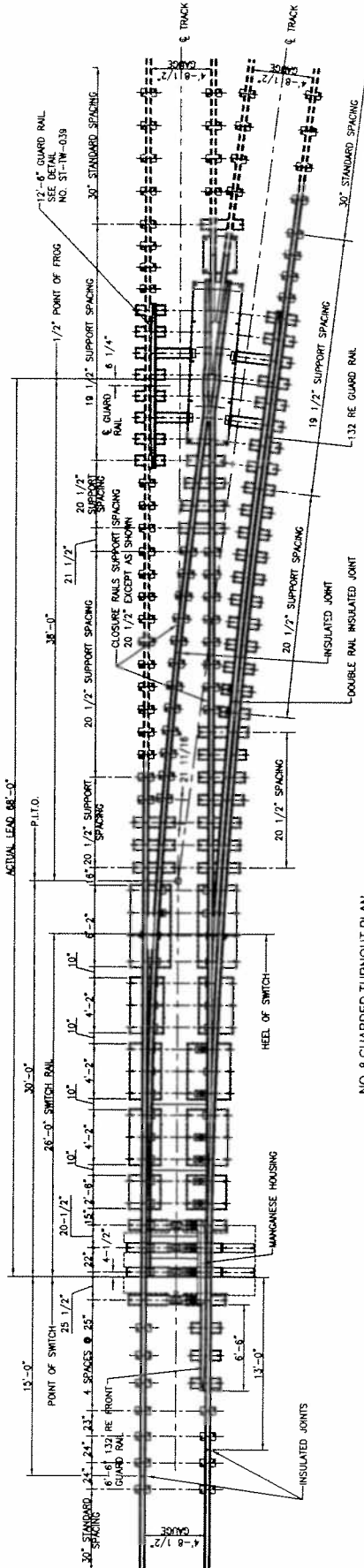
Q: Is 3.6.2 actually a requirement of inspection, or is this an error?

R: If there is a technical difficulty with the materials provided by the contractor, then the contractor will need to set up a mock switch panel at its facility so the WMATA inspection team can observe a test. One of the areas of concern is the AREMA "Samson Undertuck" where the switch point sets in the stock rail.

9. Q: What is the delivery schedule for the ordered items?

R: Replace §3.7.1 of the SOW in its entirety with the following:

"Shop drawings and submittals for items ordered must be provided to the WMATA COTR no later than thirty (30) calendar days after Notice to Proceed (NTP). Delivery of the items ordered shall be 60-90 days after NTP as agreed to by the contractor and COTR.



NO. 8 GUARDED TURNOUT PLAN
SCALE: 1/4"=1'-0"

TURNOUT DATA		FOOT	SWITCH
NUMBER		8	
ANGLE		7-09'-10"	
TOE LENGTH		6'-3"	
HEEL LENGTH		9'-5"	
TOTAL LENGTH		15'-8"	
TOE SPREAD		8'-13/16"	
HEEL SPREAD		14'-9/16"	
LENGTH OF SWITCH RAIL		28'-0"	
THICKNESS AT POINT		0"	
ANGLE AT HEEL		3'-07'-09"	
ANGLE AT POINT		0'-11'-48"	
HEEL SPREAD		9"	
HEEL POINT RADIUS		509.98'	
VERTICE DISTANCE		0	
ACTUAL LEAD		68'-0"	
STRAIGHT CLOSURE LENGTH		35'-9"	
CURVED CLOSURE LENGTH		35'-10 31/32"	
CURVED CLOSURE RADIUS		509.98'	
CENTERLINE RADIUS		507.63'	
DEGREE OF CURVE		11'-17'-13"	
TANGENT AT HEEL OF SWITCH		0"	
TANGENT AT TOE OF FROG		0"	

BILL OF MATERIAL	
QTY.	DESCRIPTION - FURNISHED TYP. PROCUREMENT
1	28'-0" GUARDED PLATED STRAIGHT SWITCH WITH UNIFORM RISERS - D.F. COMPLETE
1	28'-0" UNGUARDED PLATED SWITCH WITH UNIFORM RISERS - D.F. COMPLETE
1	12'-8" GUARD RAIL COMPLETE
1	MANGANESE HOUSING WITH DUCTILE IRON CHAIRS
6	DUCTILE IRON RAIL STOPS
A.R.	132 RE GUARD RAILS
A.R.	BOLTED STANDARD RAIL JOINTS, 6 HOLE, 16 INCH
A.R.	BOLTED INSULATED RAIL JOINTS, 6 HOLE, 36 INCH
A.R.	LEFT HAND PANOROL CLIPS #2056
A.R.	ELASTOMER PAD FOR EACH PLATE

BILL OF MATERIAL	
QTY.	DESCRIPTION - FURNISHED TYP. INSTALLATION
A.R.	DIRECT FIXATION FASTENERS
A.R.	GROUT PADS
A.R.	RESPT AND ANCHOR ASSEMBLY, ANCHORING MATERIAL, FIBROUS AS MANUFACTURED BY FOX INDUSTRIES (POLYESTER RESIN)

- NOTES:
1. GAGE WIDTH THROUGHOUT TURNOUT IS 4'-8 1/2".
 2. LOCATE SEPARATOR BLOCKS NOT MORE THAN 1/2" ON CENTERS AND AS NECESSARY TO CLEAR JOINT BARS AND ANCHOR BOLTS.
 3. JOINTS INDICATED IN RAIL LAYOUT DIAGRAM ARE REQUIRED. OTHER JOINTS LOCATED BY THE CONTRACTOR SHALL BE SUBMITTED FOR APPROVAL BY THE WAUSA ENGINEER.
 4. CLOSURE RAIL LENGTHS INDICATED ARE TOTAL RAIL LENGTHS. JOINTS SHALL BE DRILLED INTERNAL TO THE TURNOUT.
 5. RAIL ENDS CONNECTING TO OWR SHALL BE DRILLED INTERNAL TO THE RAIL LAYOUT DIAGRAM.
 6. THE CONTRACTOR SHALL FURNISH ALL JOINTS TURNOUT IS OPPOSITE HAND.
 7. RIGHT HAND TURNOUT IS SHOWN. LEFT HAND TURNOUT IS OPPOSITE HAND.
 8. JOINT IN OPPOSITE RAILS SHALL HAVE A TWO PLATE STINGER.
 9. RAIL SHALL BE HEAD HARDENED.
 10. DO NOT DRILL HOLES FOR JOINTS IN STOCK RAILS AHEAD OF POINT OF SWITCH.
 11. GUARD RAIL IS HIGH STRENGTH 132 RE SECTION. RUNNING RAIL AND COMPONENTS SHALL CONFORM TO 115 RE RAIL SECTION.
 12. ALL RAILS WITHIN THE TURNOUT ARE FASTENED TO THE PLATES BY LEFT HAND PANOROL SPRING CLIPS - #2056.
 13. RAIL AT JOINT BARS SHALL BE FASTENED WITH MODIFIED LEFT HAND PANOROL CLIP TYPE 'J' CLIPS OR 'C' CLIPS.
 14. WIDTH AND LENGTH OF ELASTOMER PADS SHALL BE 2" LARGER THAN DIMENSIONS OF CORRESPONDING PLATES.
 15. RAIL AT HEEL BLOCK SHALL BE FASTENED WITH MODIFIED LEFT HAND PANOROL CLIPS 'C' #2083 OR PANOROL 'J' CLIPS.
 16. DIMENSIONS SHOWN ON STOCK RAILS AHEAD OF P.S. ARE STANDARD AUTHORITY. ALL STOCK RAIL LENGTHS AHEAD OF P.S. WHERE OTHER THAN STANDARD LENGTHS ARE REQUIRED.
 17. SWITCH PANEL SHALL BE SHIPPED AS ONE UNIT.
 18. FOR DOUBLE RAIL INSULATED JOINTS DETAILS SEE DWG. ST-1W-041.

THIS DRAWING IS A CONTRACT DOCUMENT. IT IS THE PROPERTY OF THE CONTRACTOR AND SHALL BE KEPT UNDER THE CONTROL OF THE CONTRACTOR.

EXPIRATION DATE: _____
DRAWING NO. ST-1W-237

DESIGNED: _____ DATE: _____	DRAWN: _____ DATE: _____	CHECKED: _____ DATE: _____	SCALE: 1/4" = 1'-0"	CONTRACT NO. FC-	DRAWING NO. ST-1W-237	SHEET NO. # of #.
REFERENCE DRAWINGS			TRACKWORK STANDARD DRAWING NO. 8 GUARDED TURNOUT PLAN, PLATED DIRECT FIXATION TRACK			
REVISIONS			WASHINGTON METROPOLITAN AREA TRANSIT AUTHORITY DEPARTMENT OF TRANSIT INFRASTRUCTURE AND ENGINEERING SERVICES CENI - TRACK STRUCTURES & FACILITIES			
NO.	DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION
01	01/15/00	W. J. GILBERT	ISSUED FOR CONSTRUCTION			
02	02/07/00	M. J. KENNEDY	REVISION			
03	02/07/00	M. J. KENNEDY	REVISION			
04	02/07/00	M. J. KENNEDY	REVISION			